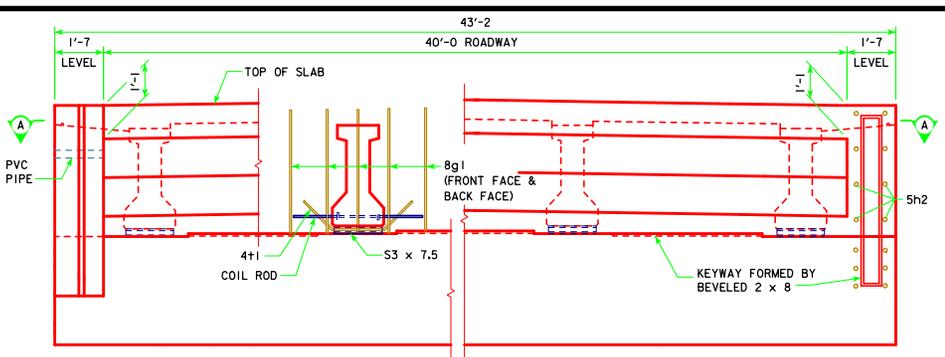
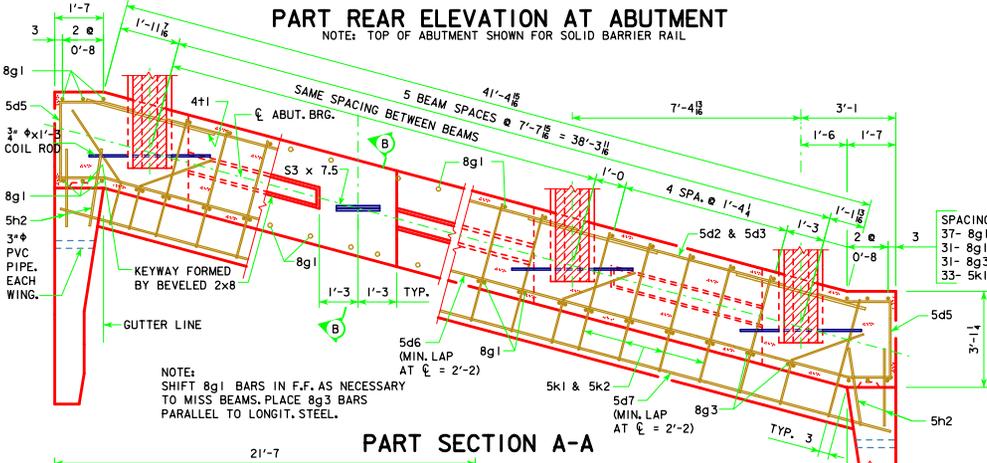


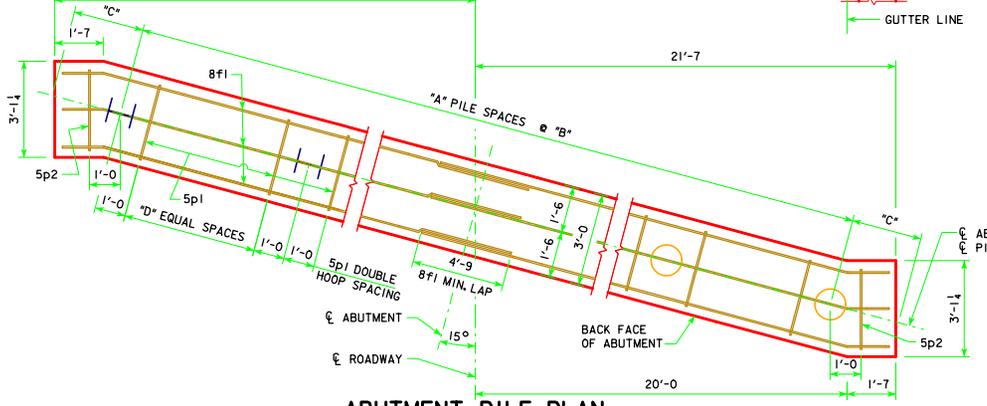
REVISED 05-13 - REVISION FOR LRFD PILE DESIGN.



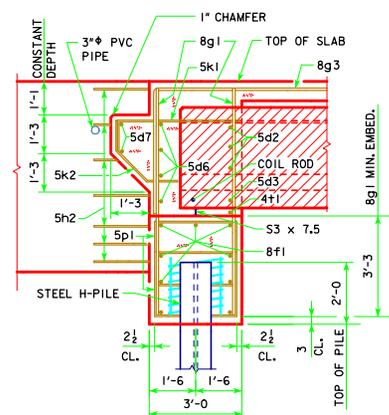
PART REAR ELEVATION AT ABUTMENT
NOTE: TOP OF ABUTMENT SHOWN FOR SOLID BARRIER RAIL



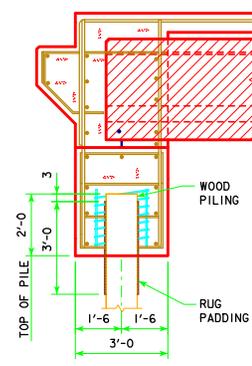
PART SECTION A-A



ABUTMENT PILE PLAN



PART SECTION B-B (FOR STEEL H-PILE)



PART SECTION B-B (FOR WOOD PILING)

WOOD PILING NOTE:
AFTER PILES ARE CUT OFF, THE UPPER 3', EXCEPT AS SHOWN, IS TO BE WRAPPED WITH A DOUBLE THICKNESS OF RUG PADDING HELD IN PLACE BY TACKING WITH GALVANIZED ROOFING NAILS AND WRAPPED WITH #14 GAUGE GALVANIZED WIRE AT A 4" PITCH, CARE IS TO BE TAKEN NOT TO DAMAGE PADDING WHEN PLACING CONCRETE. RUG PADDING MAY BE EITHER OF THE FOLLOWING:
(1) HAIR AND JUTE RUG PADDING, RUBBERIZED ON BOTH SIDES, AND WEIGHING NOT LESS THAN 47 OZ. PER SQ. YD.
(2) BONDED URETHANE OR BONDED POLYFOAM WITH A MINIMUM DENSITY OF 5 LBS. PER CU. FT. AND SHALL BE AT LEAST 1/2 IN. THICK, (MATERIAL LESS THAN 1/2 IN. IN THICKNESS MAY BE USED, BUT WILL REQUIRE ADDITIONAL WRAPS FOR A TOTAL OF AT LEAST ONE INCH).

NOTE: THE SPIRAL AT THE TOP OF EACH PILE TO BE 7 TURNS OF NO. 2 BAR, 2 1/4" DIAMETER, 3" PITCH WITH 3 - 1 1/2" x 1/2" x 1/2" SPACERS PUNCHED TO HOLD SPIRAL.

SPACING FOR:
37- 8g1 BACK FACE
31- 8g1 FRONT FACE
31- 8g3 BACK FACE
33- 5k1 & 5k2 BACK FACE

ABUTMENT NOTES:

MINIMUM CLEAR DISTANCE FROM FACE OF CONCRETE TO NEAR REINFORCING BAR IS TO BE 2" UNLESS OTHERWISE NOTED OR SHOWN.

IF NECESSARY TO PREVENT DAMAGE TO THE END OF THE BRIDGE DECK OR BACKWALL FROM CONSTRUCTION EQUIPMENT, AN APPROPRIATE METHOD OF PROTECTION APPROVED BY THE ENGINEER SHALL BE PROVIDED BY THE BRIDGE CONTRACTOR AT NO EXTRA COST TO THE COUNTY OR STATE.

ABUTMENT PILES SHALL BE DRIVEN TO VALUES SHOWN IN DESIGN PLANS.

PLACE 5h2 BAR AT 1:6 SLOPE TO MATCH TRAFFIC SIDE OF ABUTMENT WING FACE. (BOTH SIDES TYPICAL)

BARRIER RAIL NOT SHOWN IN DETAILS.

IF ROCK IS CLOSER THAN 15' BELOW ABUTMENT FOOTING, SPECIAL ANALYSIS MAY BE REQUIRED.

ABUTMENT PILE SPACING		ABUT. BRG.				
		138'-10	151'-4	163'-10	176'-4	188'-10
WITH WOOD PILES	*A* PILE SPACES	13	14	15	15	16
	B (FT. - IN.)	3'-0	2'-10	2'-8	2'-8	2'-6
	C (FT. - IN.)	2'-10 1/2	2'-6 1/2	2'-4 1/2	2'-4 1/2	2'-4 1/2
	D EQUAL SPACES	1	1	1	1	1
	NO. OF PILES PER ABUT.	14	15	16	16	17
PU, STRENGTH 1 DESIGN LOAD (KIPS)		57	55	55	57	56
WITH STEEL H-PILES	*A* PILE SPACES	6	6	6	6	7
	B (FT. - IN.)	6'-8	6'-8	6'-8	6'-8	5'-8
	C (FT. - IN.)	2'-4 1/2	2'-4 1/2	2'-4 1/2	2'-4 1/2	2'-6 1/2
	D EQUAL SPACES	4	4	4	4	3
	NO. OF PILES PER ABUT.	7	7	7	7	8
PU, STRENGTH 1 DESIGN LOAD (KIPS)		124	129	138	143	129

NOTE: PU, STRENGTH 1 DESIGN LOAD (KIPS) IS NOT THE VALUE USED IN THE FIELD FOR DRIVING PILES.

LATEST REVISION DATE
05-13
APPROVED BY BRIDGE ENGINEER
Thomas E. Mc Donnell

Iowa Department of Transportation
Highway Division

STANDARD DESIGN - 40' ROADWAY, THREE SPAN BRIDGE
PRETENSIONED PRESTRESSED CONCRETE BEAM BRIDGES
AUGUST, 2009

ABUTMENT DETAILS 15° SKEW A & B BEAMS	H40-11-06
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